



4/14/2017

VIA EMAIL

Miriam Lim, Junior Planner
City of Sacramento Community Development Department
300 Richards, 3rd Floor
Sacramento, CA 95811

RE: Mack & Franklin Development (P17-016)

Dear Ms. Lim:

WALKSacramento has reviewed the project routing for Mack & Franklin Development (P17-016). The project proposes to construct a convenience store with a future attached carwash and 18 fuel dispensers, a fast food restaurant, a medical office building, an office building and associated surface parking. The site is on the Franklin Point shopping center property at the southwest corner of Mack and Franklin where there is an existing Family Dollar Store at the corner. The proposed project would replace the approved Franklin Point (P05-153) site plan.

The Mack & Franklin Development project provides no pedestrian access to the proposed buildings and no walkways between any of the buildings, even Buildings 2 and 3 whose front sidewalks are separated by less than 5 feet of landscaping. The site plan for P05-153 was far superior to the one proposed for the Mack & Franklin Development project.

Figure 1 shows a markup of the Franklin Point site plan. Walkways through the parking lot, across driveways and the connection to Villa Terrassa residential neighborhood to the south (bottom), are shaded with a light blue color; sidewalks around buildings and along the streets are indicated with purple lines. Pedestrian circulation within the shopping center (exclusive of the Family Dollar Store which replaced a proposed convenience store with fuel dispensers) and connections to Mack Road, Franklin Boulevard and Villa Terrassa are conveniently located.

The importance of the pedestrian circulation for Franklin Point is demonstrated by several conditions of approval for the project, one of which reads as follows (without the specific conditions included): *E-1. I. The Site and Landscape plans shall be revised and refined to enhance pedestrian connections and walkability throughout the site. Specific areas to be refined are between the convenience market and Franklin Blvd. and between the freestanding restaurant and the office buildings. This will involve the reduction of non-required parking spaces. The specific requirements are ...*

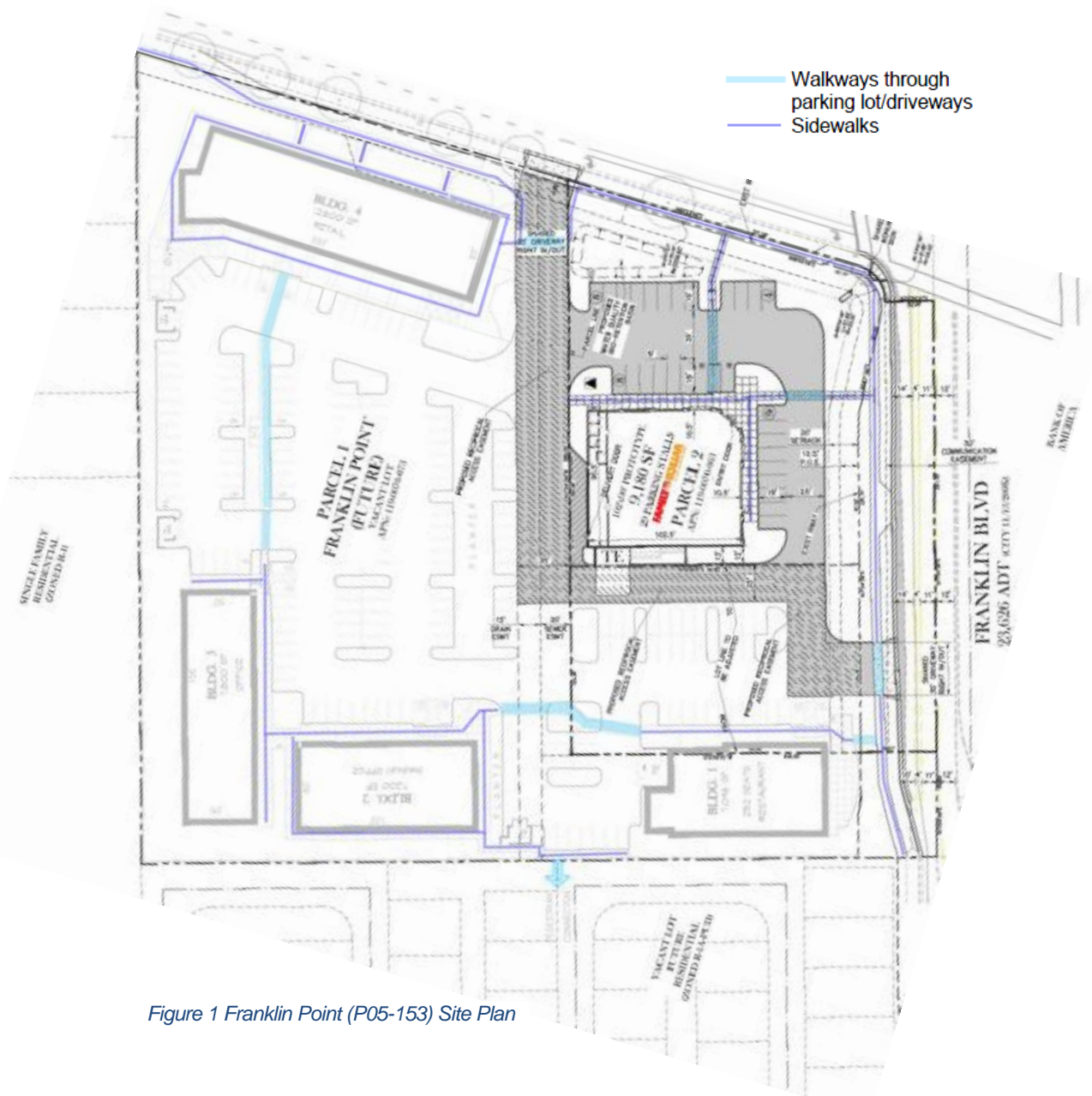


Figure 1 Franklin Point (P05-153) Site Plan

Figure 2 shows a markup of the proposed site plan for the Mack and Franklin Development where missing walkways on the site are shown with orange lines. These walkways are critical to provide safe access to the new businesses, whether pedestrians are arriving from surrounding neighborhoods or by transit, or if they just want to move between buildings.

Note that the pedestrian connection to the Villa Terrassa neighborhood is not included. Given the size and arrangement of the Villa Terrassa street network, providing this connection may not provide much benefit.

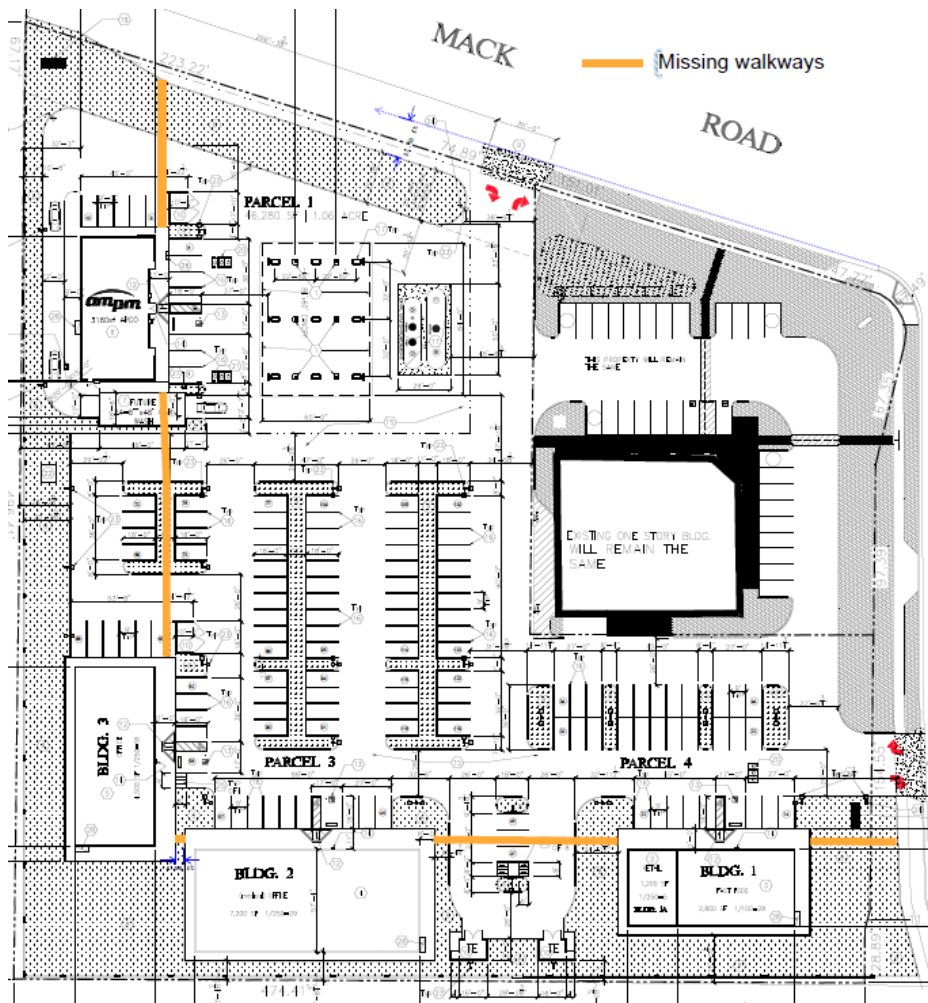


Figure 2 Mack & Franklin Development Site Plan

The car wash proposed for the convenience store presents several impediments to pedestrian circulation. First, car wash structure extends beyond the sidewalk in front of the convenience store, interrupting the line-of-sight and movement between the convenience store and building 3 to the south. Second, if there's a walkway in front of the car wash exit, pedestrians crossing the car wash driveway may be unable to hear warning sounds or voices due to the noise of the dryer fans, and cars slowly exiting may block the crosswalk.

We have a concern with the frontage along Mack Road. The site plan doesn't show a sidewalk for the south side of Mack Road between the driveway and the west edge of the property. We would expect that a detached sidewalk with shade trees behind the back of sidewalk will be constructed. Shade trees in the landscape planter between the street and sidewalk should also be provided.

Finally, a landscape plan wasn't included with the routing. Trees are an important element of the pedestrian environment, and we would like to verify that adequate shading is provided for pedestrians.

WALKSacramento is working to support increased physical activity such as walking and bicycling in local neighborhoods as well as helping to create community environments that support walking and bicycling. The benefits include improved physical fitness, less motor vehicle traffic congestion, better air quality, and a stronger sense of cohesion and safety in local neighborhoods.

Thank you for your consideration of these comments and recommendations. If you have questions or need additional information, please contact me at (916) 446-9255 or cholm@walksacramento.org.

Sincerely,

Chris Holm
Project Manager

Attachment: Development Checklist for Biking and Walking

Enclosures: Site plan Mack & Franklin markup.pdf

Site plan Family Dollar on Franklin Point from P13-034_ROD.pdf

DEVELOPMENT CHECKLIST for BIKING and WALKING

*Prepared by WALKSacramento and SABA (Sacramento Area Bicycle Advocates)
September 2012*

This checklist is provided to give an indication of design, engineering, and policy elements that we consider when reviewing development projects.

POLICIES

- Walking and biking is a priority
- Adopted a policy to develop a full multi-modal and ADA accessible transportation system

Project Review and Comment

POLICY CONSIDERATIONS

- Pedestrian Master Plan
- Bicycle Master Plan
- Regional Blueprint
- Regional Blueprint Consistent General Plans
- Adopted Climate Action Plans
- Subdivision ordinances to support pedestrian and bicycle access and safety
- Zoning ordinance to support pedestrian and bicycle access and safety

ENGINEERING

- SIDEWALKS & BIKELANES ON BOTH SIDES OF MAJOR ROADWAYS**
 - Pedestrian Level of Service "C" or better on arterials
 - Bicycle Level of Service "C" or better on arterials
- SAFE CROSSINGS FOR PEDESTRIANS**
 - every 300-600 feet on major arterials
 - well lit, marked crosswalks
 - audible signals & count-down signals
 - median refuge islands
- SPEED MANAGEMENT**
 - Speed limits based on safety of pedestrians and bicyclists
 - Implement "road diets" where there is excess lane capacity
- STREET DESIGN STANDARDS**
 - Maximize pedestrian and bicyclist safety
 - Sidewalks buffered by trees and landscaping on major arterials
 - Vertical curbs
 - 5' minimum sidewalk widths, 8' in front of schools
 - 6' minimum bike lanes on busy streets

- ❑ INTERSECTIONS
 - Median refuge islands for pedestrians
 - Signal timing to enable safe passage
 - Signal detection for bicyclists
 - Crossings on all 4 legs of intersections

- ❑ ELIMINATE BARRIERS
 - Freeway, railroad, river and creek crossings
 - Obstructions in sidewalks and bike lanes

NEW DEVELOPMENT – REQUIRE

- ❑ Walking & bicycling circulation plans for all new development
- ❑ Direct and convenient connections to activity centers, including schools, stores, parks, transit
- ❑ Mixed uses and other transit supporting uses within ¼ mile of light rail stations or bus stops with frequent service
- ❑ Minimum width streets
- ❑ Maximum block length of 400'
- ❑ 4-lane maximum for arterials; Recommend 2 lanes wherever possible

NEW DEVELOPMENT – DISCOURAGE

- ❑ Cul-de-sacs (unless it includes bike/ped connections)
- ❑ Gated and/or walled communities
- ❑ Meandering sidewalks
- ❑ Inappropriate uses near transit (gas stations, drive-thru restaurants, mini storage and other auto dependent uses)

BUILDINGS – REQUIRE

- ❑ Direct access for pedestrians from the street
- ❑ Attractive and convenient stairways
- ❑ Bicycle parking – long & short term
- ❑ Shower & clothing lockers

OLDER NEIGHBORHOODS

- ❑ Improve street crossings
- ❑ Reduce speeds
- ❑ Provide new connections
- ❑ Create short cuts for walkers and bicyclists by purchase of properties or other means
- ❑ Provide sidewalks on both sides of major streets

Policy Review and Comment

ENFORCEMENT & MAINTENANCE

- ❑ Enforce speed limits
- ❑ Enforce crosswalk rules – conduct crosswalk sting operations
- ❑ Enforce restrictions against parking on sidewalks
- ❑ Enforce bicycle rules including riding with traffic, lights at night, stopping at red lights
- ❑ Implement CVC 267 setting speed limits based on pedestrian and bicyclist safety
- ❑ Sweep streets and fix hazards
- ❑ Repair and replace broken sidewalks

EDUCATION

- ❑ Train staff on pedestrian and bicycle facility design.
- ❑ Train development community about pedestrian and bicycle planning and safety issues
- ❑ Bicycle skills training

FUNDING

- ❑ Include pedestrian and bicycle facilities in capital improvement programs
- ❑ Include pedestrian and bicycle facilities as a part of roadway widening and improvement projects
- ❑ Support Measure A pedestrian and bicycle facility allocation
- ❑ Set priorities based on safety and latent demand
- ❑ SACOG Community Design grants & Bike/Ped grants
- ❑ California Bicycle transportation Account
- ❑ Safe Routes to School

www.walksacramento.org

WALKSacramento
909 12th Street, Suite 203
Sacramento, CA 95814
(916) 446-9255

www.sacbike.org

Sacramento Area Bicycle Advocates
909 12th Street, Suite 116
Sacramento, CA 95814
(916) 444-6600