



10/28/2016

VIA EMAIL

Miriam Lim, Junior Planner
City of Sacramento community Development Department
300 Richards Boulevard, 3rd Floor
Sacramento, Ca 95811

RE: Tran Villa Apartments TM & SPDR (Z16-055)

Dear Ms. Lim:

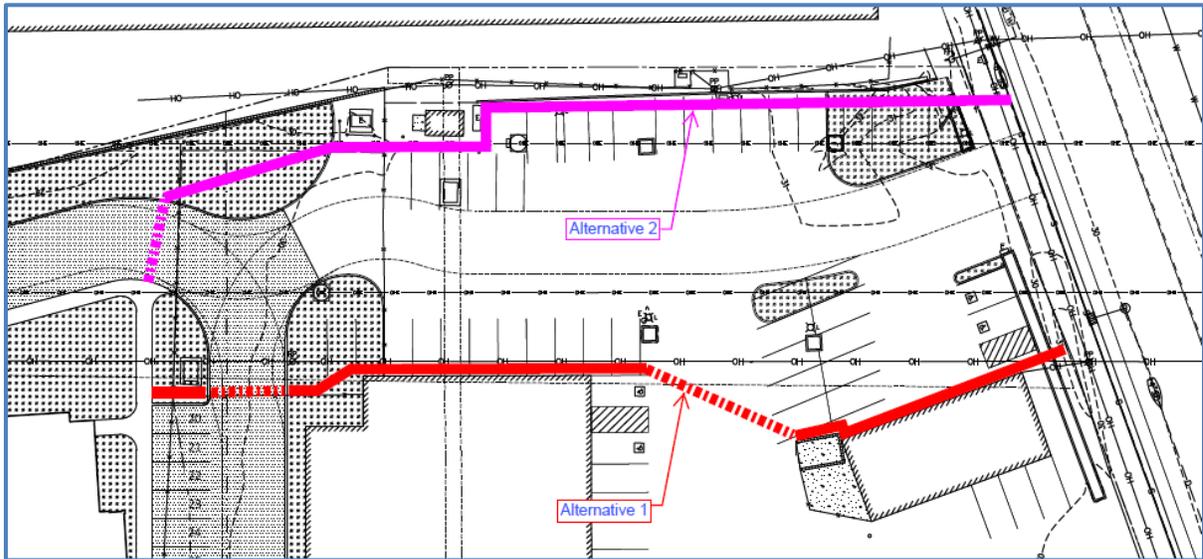
WALKSacramento has reviewed the September 29, 2016 routing for Tran Villa Apartments TM & SPDR (Z16-055) and offer the following comments.

The Tran Villa Apartments project proposes to build a 32-unit apartment complex comprised of two 2-story buildings and parking for 40 vehicles on a 1.88-acre vacant area behind an existing shopping center at 6458 Stockton Boulevard. The project site is on a commercial corridor with numerous restaurant and businesses, and it's within a 10-minute walk of a major shopping center. There's also a bus stop for Route 51, a high-frequency and high-ridership route, immediately in front of the property.

Development projects that lead to more walking and active travel are critical to our community's future. Human beings need moderate exercise, such as walking, for about 150 minutes per week to prevent the development of chronic disease and overweight, yet only 30% of the population in the Sacramento region is active at this minimal level, often due to limitations placed by a built environment not suited to walking and other types of physically active travel. Stockton Boulevard has infrequent pedestrian crossings, but it does have sidewalks with vertical curbs separated from traffic lanes by bicycle lanes. The sidewalk in front of the shopping center on the project parcel is extra wide and has several trees. For these reasons, walking to daily destinations and transit should be very common for the future residents of Tran Villa Apartments. However, the proposed pedestrian access to the apartments is inadequate and will not be safe.

The pedestrian circulation section of the City of Sacramento *Multi-Family Design Guidelines* includes the following important elements that are missing from the proposed site plan: 1) pedestrian planning should be on par with vehicular access, 2) pedestrian access should not be limited to vehicle access locations, 3) sidewalks should not be combined with or be a part of driveways, 4) pedestrian pathways should be separated from auto circulation and conflict points emphasized with changes in grade, materials, textures or colors, and 5) pedestrian pathways should include amenities such as trellises, trees/landscaping, and lighting.

WALKSacramento recommends the following revision to the site plan to provide a safe pedestrian route between the apartments and the public sidewalk. **Add a raised sidewalk along the north sides of the two northern-most buildings in the shopping center and a raised crosswalk across the drive aisle on the apartment site and in the shopping center, or add a sidewalk with landscape planters on both sides that extends between the two landscape planters proposed for the north edge of the parcel and a raised crosswalk across the drive aisle on the apartment site.** See diagram below and the attached drawing. The landscape planter between the sidewalk and the parking lot should have trees to provide a shaded walkway and some parking lot shading.



WALKSacramento is working to support increased physical activity such as walking and bicycling in local neighborhoods as well as helping to create community environments that support walking and bicycling. The benefits include improved physical fitness, less motor vehicle traffic congestion, better air quality, and a stronger sense of cohesion and safety in local neighborhoods.

Thank you for your consideration of these comments and recommendations. If you have questions or need additional information, please contact me at (916) 446-9255 or cholm@walksacramento.org.

Sincerely,
Chris Holm
Project Manager

Enclosure: Development Checklist for Biking and Walking
Attachment: Site plan 2016-08 Tran Villa Apartments markup

DEVELOPMENT CHECKLIST for BIKING and WALKING

*Prepared by WALKSacramento and SABA (Sacramento Area Bicycle Advocates)
September 2012*

This checklist is provided to give an indication of design, engineering, and policy elements that we consider when reviewing development projects.

POLICIES

- Walking and biking is a priority
- Adopted a policy to develop a full multi-modal and ADA accessible transportation system

Project Review and Comment

POLICY CONSIDERATIONS

- Pedestrian Master Plan
- Bicycle Master Plan
- Regional Blueprint
- Regional Blueprint Consistent General Plans
- Adopted Climate Action Plans
- Subdivision ordinances to support pedestrian and bicycle access and safety
- Zoning ordinance to support pedestrian and bicycle access and safety

ENGINEERING

- SIDEWALKS & BIKELANES ON BOTH SIDES OF MAJOR ROADWAYS**
 - Pedestrian Level of Service "C" or better on arterials
 - Bicycle Level of Service "C" or better on arterials
- SAFE CROSSINGS FOR PEDESTRIANS**
 - every 300-600 feet on major arterials
 - well lit, marked crosswalks
 - audible signals & count-down signals
 - median refuge islands
- SPEED MANAGEMENT**
 - Speed limits based on safety of pedestrians and bicyclists
 - Implement "road diets" where there is excess lane capacity
- STREET DESIGN STANDARDS**
 - Maximize pedestrian and bicyclist safety
 - Sidewalks buffered by trees and landscaping on major arterials
 - Vertical curbs
 - 5' minimum sidewalk widths, 8' in front of schools
 - 6' minimum bike lanes on busy streets

- ❑ **INTERSECTIONS**
 - Median refuge islands for pedestrians
 - Signal timing to enable safe passage
 - Signal detection for bicyclists
 - Crossings on all 4 legs of intersections

- ❑ **ELIMINATE BARRIERS**
 - Freeway, railroad, river and creek crossings
 - Obstructions in sidewalks and bike lanes

NEW DEVELOPMENT – REQUIRE

- ❑ Walking & bicycling circulation plans for all new development
- ❑ Direct and convenient connections to activity centers, including schools, stores, parks, transit
- ❑ Mixed uses and other transit supporting uses within ¼ mile of light rail stations or bus stops with frequent service
- ❑ Minimum width streets
- ❑ Maximum block length of 400'
- ❑ 4-lane maximum for arterials; Recommend 2 lanes wherever possible

NEW DEVELOPMENT – DISCOURAGE

- ❑ Cul-de-sacs (unless it includes bike/ped connections)
- ❑ Gated and/or walled communities
- ❑ Meandering sidewalks
- ❑ Inappropriate uses near transit (gas stations, drive-thru restaurants, mini storage and other auto dependent uses)

BUILDINGS – REQUIRE

- ❑ Direct access for pedestrians from the street
- ❑ Attractive and convenient stairways
- ❑ Bicycle parking – long & short term
- ❑ Shower & clothing lockers

OLDER NEIGHBORHOODS

- ❑ Improve street crossings
- ❑ Reduce speeds
- ❑ Provide new connections
- ❑ Create short cuts for walkers and bicyclists by purchase of properties or other means
- ❑ Provide sidewalks on both sides of major streets

Policy Review and Comment

ENFORCEMENT & MAINTENANCE

- ❑ Enforce speed limits
- ❑ Enforce crosswalk rules – conduct crosswalk sting operations
- ❑ Enforce restrictions against parking on sidewalks
- ❑ Enforce bicycle rules including riding with traffic, lights at night, stopping at red lights
- ❑ Implement CVC 267 setting speed limits based on pedestrian and bicyclist safety
- ❑ Sweep streets and fix hazards
- ❑ Repair and replace broken sidewalks

EDUCATION

- ❑ Train staff on pedestrian and bicycle facility design.
- ❑ Train development community about pedestrian and bicycle planning and safety issues
- ❑ Bicycle skills training

FUNDING

- ❑ Include pedestrian and bicycle facilities in capital improvement programs
- ❑ Include pedestrian and bicycle facilities as a part of roadway widening and improvement projects
- ❑ Support Measure A pedestrian and bicycle facility allocation
- ❑ Set priorities based on safety and latent demand
- ❑ SACOG Community Design grants & Bike/Ped grants
- ❑ California Bicycle transportation Account
- ❑ Safe Routes to School

www.walksacramento.org

www.sacbike.org

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